

NOTES:

- COST FOR MOBILIZATION AND DEMOBILIZATION TO BE INCLUDED IN ITEMS BID FOR HAULING AND PLACING OF ASPHALT CONCRETE.
- CONTRACTOR TO PLACE TEMPORARY PAVEMENT MARKINGS AFTER PLACING ASPHALT CONCRETE. TEMPORARY PAVEMENT MARKINGS CONSIST OF A STRIPE OF YELLOW PAVEMENT TAPE LONG PLACED EVERY 40'. COST OF TEMPORARY PAVEMENT MARKINGS TO BE INCLUDED IN ITEM 404.12720108
- ALBANY COUNTY D.P.W. TO PROVIDE WZTC FOR HAULING AND PLACING OF ASPHALT CONCRETE. EXCEPT FOR CR. 151 and CR 154 WORK. CONTRACTOR IS RESPONSIBLE FOR WZTC ON CR. 151 and CR 154.
- CONTRACTOR TO COLD MILL A PAVEMENT REBATE PER ACS-42 (TERMINATION DETAIL FOR PAVEMENT OVERLAYS) UNDER ITEM 404.XXXXXX
- ITEM 404.XXXXXX TO BE PLACED AS DIRECTED BY THE COMMISSIONER OF PUBLIC WORKS OR HER REPRESENTATIVE.
- EXCEPT AS MODIFIED IN THE CONTRACT PLANS AND IN THE MODIFIED PROPOSAL, ALL WORK CONTEMPLATED UNDER THIS CONTRACT SHALL CONFORM TO THE REQUIREMENTS OF THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS DATED JANUARY 1, 2025. WITH CURRENT ADDITIONS AND MODIFICATIONS. THE CONSTRUCTION, METHOD OF MEASUREMENT AND BASIS OF PAYMENT OF ALL WORK CONTEMPLATED UNDER THIS CONTRACT SHALL BE PERFORMED USING U.S. CUSTOMARY UNITS.
- CONTRACTOR TO BE RESPONSIBLE FOR PROVIDING A CERTIFIED AND METERED VEHICLE FOR THE APPLICATION OF TACK COAT.
- CONTRACTOR SHOULD BE AWARE OF EXISTING ASPHALT DRIVEWAYS ON VARIOUS COUNTY ROADS INCLUDED IN THIS CONTRACT. ALBANY COUNTY WILL PROVIDE REBATES IN SAID DRIVEWAYS. CONTRACTOR TO BE RESPONSIBLE FOR PLACING AND COMPACTING THE TOP COURSE ASPHALT (APPROX. 2" THICK) USING SUITABLE EQUIPMENT ABOVE. COST TO BE INCLUDED UNDER ITEM # 608.020102
- UNDER CONTEMPLATION OF THIS CONTRACT, CONTRACTOR TO BE ADVISED THAT ALL MATERIALS GENERATED UNDER ITEM# 490.10 ARE THE SOLE PROPERTY OF ALBANY COUNTY AND ALL RESPONSIBILITIES FOR REMOVAL AND STORAGE SHALL BE MET ACCORDINGLY.
- CONTRACTOR SHALL BE AWARE THAT ALBANY COUNTY IS SPECIFYING 70 SERIES COMPACTION UNDER THIS CONTRACT AND THE CONTRACTOR IS RESPONSIBLE FOR ALL TESTING REQUIREMENTS IN THE APPLICABLE SPECIFICATION.

ALBANY COUNTY – 2025

**PROJECT NO. 25-C591
 BID # 2025 - 001
 HAULING & PLACING ASPHALT CONCRETE
 ON VARIOUS COUNTY ROADS
 HIGHWAY IMPROVEMENT PROJECT**

MILL REBATES
 PRODUCTION MILL 2" THICK, PAVE TOP COURSE 2" THICK, PAVEMENT STRIPING

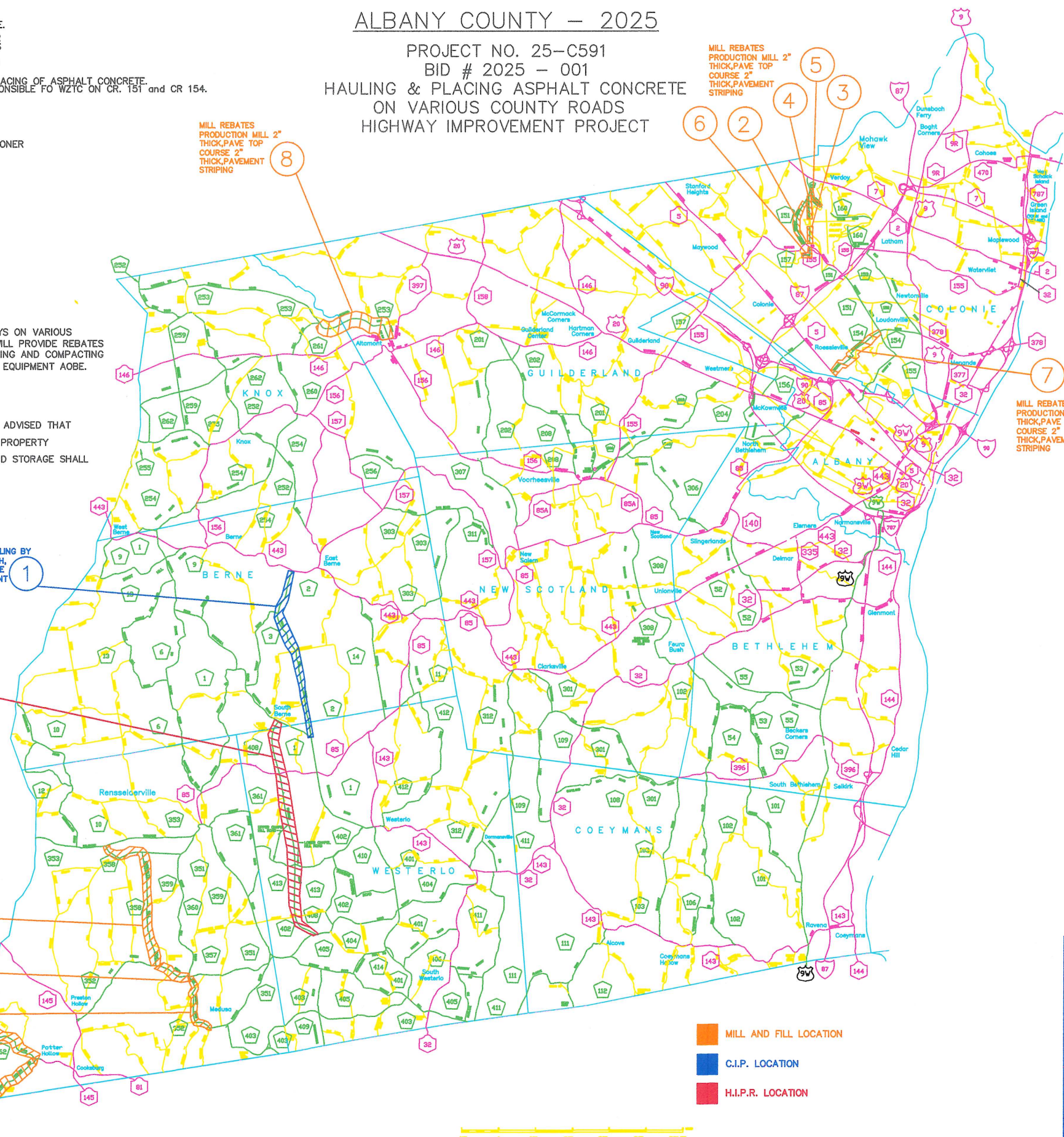
MILL REBATES
 PRODUCTION MILL 2" THICK, PAVE TOP COURSE 2" THICK, PAVEMENT STRIPING

MILL REBATES
 PAVEMENT RECYCLING BY OTHERS, 4" DEPTH, PAVE TOP COURSE 2" THICK, PAVEMENT STRIPING

MILL REBATES
 PAVEMENT RECYCLING BY OTHERS, 1.5" DEPTH, PAVE TOP COURSE 2" THICK, PAVEMENT STRIPING

MILL REBATES
 PRODUCTION MILL 2" THICK, PAVE TOP COURSE 2" THICK, PAVEMENT STRIPING

MILL REBATES
 PRODUCTION MILL 2" THICK, PAVE TOP COURSE 2" THICK, PAVEMENT STRIPING



SITE	TOWN	COUNTY ROUTE	DESCRIPTION	LENGTH (MILES)	TONS±
1.	BERNE	2	FROM S.R. 443 TO CR. 1	4.8 MI.	8,140 TNS.
2.	COLONIE	151	FROM TERMINAL RD TO AIRPORT PARK BLVD	1.4 MI.	4,720 TNS.
3.	COLONIE	HOCKEY LANE	FROM DEAD END TO AIRPORT TERM RD	0.14 MI.	300 TNS.
4.	COLONIE	OLD ALB SHKR RD	FROM CR 151 TO DEAD END	0.5 MI.	1,070 TNS.
5.	COLONIE	160	FROM CR 151 TO DEAD END	0.6 MI.	1,080 TNS.
6.	COLONIE	HERITAGE LANE	FROM CR 151 TO DEAD END	0.6 MI.	1,080 TNS.
7.	COLONIE	154	FROM CR151 TO S.R. 5	1.98 MI.	3,320 TNS.
8.	GUILDERLAND/ KNOX	253	FROM CR 261 TO S.R. 146	2.4 MI.	3,900 TNS.
9.	WESTERLO	408	FROM CR 1 TO CR 402	5.96 MI.	9,890 TNS.
10.	RENSSELAERVILLE	352	FROM CR 358 TO CR 360	1.05 MI.	1,840 TNS.
11.	RENSSELAERVILLE	352	FROM CR 357 TO CR 351	1.79 MI.	2,860 TNS.
12.	RENSSELAERVILLE	358	FROM CR 353 TO CR 352	4.3 MI.	7,190 TNS.
13.	RENSSELAERVILLE	354	FROM CR 362 TO COUNTY LINE	1.83 MI.	2,660 TNS.
14.	RENSSELAERVILLE	362	FROM COUNTY LINE TO CR 354	2.08 MI.	3,040 TNS.
TOTALS:				29.43 MI.	51,090 TNS.

HAULING AND PLACING ASPHALT CONCRETE		
ITEM NUMBER	DESCRIPTION	QTY.
404.37790108	37.5 MM. F9, BASE COURSE, 70 SERIES COMPACTION	250 TNS.
404.19790108	19 MM. F9, BINDER 70 SERIES COMPACTION	250 TNS.
404.12720108	12.5 MM. F2, TOP COURSE 70 SERIES COMPACTION	51,090 TNS.
404.058901	F9, SHIM COURSE	250 TNS.
407.0101	TACK COAT	23,220 GAL.
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	300,370 SQ YDS
490.30	MISC. COLD MILLING OF BITUMINOUS CONCRETE	2,080 SQYDS
604.07XXYY	ALTERING DRAINAGE STRUCTURES	1 EACH
604.10	PREFABRICATED ADJUSTMENTS RINGS FOR MANHOLES	10 EACH
608.020102	HOT MIX ASPHALT HANDWORK FOR DRIVEWAYS	60 TONS
619.0101	BASIC WORK ZONE TRAFFIC CONTROL (DAILY OPERATION, CR 151 & 154 ONLY)	LUMP SUM
640.22	REFLECTORIZED PAINT CHARACTERS	56 EACH
640.23	REFLECTORIZED PAINT SYMBOLS	22 EACH
640.20	PAVEMENT STRIPES WHITE PAINT REFLECTORIZED-20 MIL	263,222 LF
640.21	PAVEMENT STRIPES YELLOW PAINT REFLECTORIZED-20 MIL	263,222 LF
680.54	INDUCTANCE LOOP INSTALLATION	1,345 LF
680.72	INDUCTANCE LOOP WIRE	4,465 LF
697.01	INTERIM PAYMENT	LS
685.1102	WHITE EPOXY REFLECTORIZED STRIPES - 20 MIL	73,315 LF
685.1202	YELLOW EPOXY REFLECTORIZED STRIPES - 20 MIL	48,404 LF
685.3304	WHITE EPOXY REFLECTORIZED PAVEMENT LETTERS - 20 MIL	16 EA
685.3404	WHITE EPOXY REFLECTORIZED PAVEMENT SYMBOLS - 20 MIL	13 EA

PAVEMENT RECYCLING BY OTHERS N.I.C.					
SITE	TOWN	ROUTE	DEPTH	QTY(SQ YDS)	
1.	BERNE	2	FROM S.R. 443 TO CR 1	4"	73,860 SQYDS
9.	WESTERLO	408	FROM CR 1 TO CR 402	1.5"	88,170 SQYDS
				TOTAL:	162,030 SQYDS

It is a violation of the New York State Education Law for any person, unless acting under the direction of a Licensed Professional Engineer, to offer in any way any plans, specifications, plots or reports to which the seal of a Professional Engineer has been applied.

COUNTY OF ALBANY
DEPARTMENT OF PUBLIC WORKS
 449 NEW SALEM ROAD
 VOORHEESVILLE, NEW YORK 12186
 518-765-2786

LOCATION MAP

HAULING AND PLACING ASPHALT CONCRETE AND MISCELLANEOUS PAVING HIGHWAY IMPROVEMENT PROJECT ALBANY COUNTY

DRAWN BY: NJC	SCALE: AS SHOWN	DRAWING NUMBER: HP-25
CHECKED BY: BRM	DATE: 2025	PROJ. I.D. NO.: 25-C591
APPROVED BY: BRM		SHEET 1 OF 4

ALBANY COUNTY – 2025

PROJECT NO. 25-C591
 BID #2025-001

HAULING & PLACING ASPHALT CONCRETE ON VARIOUS COUNTY ROADS HIGHWAY IMPROVEMENT PROJECT

GENERAL NOTES

1. UNLESS OTHERWISE NOTED ALL ITEM NUMBERS FOR SPECIFIC WORK TASKS RELATE TO THE NEW YORK STATE STANDARD SPECIFICATIONS DATED JANUARY 1, 2025.
2. LOCATION OF UTILITIES, PUBLIC AND/OR PRIVATE, INDICATED ON THE PLANS AS EXISTING AND/OR TO BE CONSTRUCTED ARE APPROXIMATE ONLY. THEIR EXACT LOCATION SHALL BE DETERMINED IN THE FIELD. ADDITIONAL UTILITY LINES, WHETHER ABANDONED OR IN SERVICE, MAY EXIST AND IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONDUCT HIS OPERATIONS, AND TAKE NECESSARY PRECAUTIONS SUCH THAT INTERFERENCE WITH OR DAMAGE TO THESE OR OTHER FACILITIES DURING THE COURSE OF CONSTRUCTION IS PREVENTED.
3. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL CALL DIG SAFE N.Y. (1-800-962-7962) TO HAVE UNDERGROUND UTILITIES LOCATED.
4. IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY SERVICE CAUSING AN INTERRUPTION IN SAID SERVICE, HE SHALL IMMEDIATELY COMMENCE WORK TO RESTORE THAT SERVICE AT HIS OWN EXPENSE. HE MAY NOT CEASE HIS WORK OPERATION UNTIL THAT SERVICE IS RESTORED.
5. THE CONTRACTOR SHALL EXAMINE AND VERIFY IN THE FIELD ALL EXISTING AND GIVEN CONDITIONS, ELEVATIONS AND DIMENSIONS SHOWN ON THE PLANS. IF FIELD CONDITIONS AND DIMENSIONS DIFFER FROM THOSE SHOWN ON THE PLANS, THE CONTRACTOR SHALL MAKE APPROPRIATE CHANGES TO THOSE SHOWN ON THE PLANS, AS APPROVED BY THE ENGINEER. ALL FIELD CONDITIONS AND DIMENSIONS SHALL BE SO NOTED ON THE AS-BUILT DRAWINGS SUBMITTED FOR APPROVAL.
6. THERE SHALL BE NO CLAIM MADE BY THE CONTRACTOR FOR WORK PERTAINING TO SUCH MODIFICATIONS AS MAY BE REQUIRED DUE TO DIFFERENCES BETWEEN ACTUAL FIELD CONDITIONS AND THOSE SHOWN BY THE DETAILS AND DIMENSIONS ON THE PLANS. THE CONTRACTOR SHALL BE PAID FOR ACTUAL QUANTITIES OF MATERIALS USED, OR FOR THE ACTUAL WORK PERFORMED, UNDER VARIOUS ITEMS IN THE CONTRACT AT THE UNIT PRICE BID FOR THOSE ITEMS.
7. THE CONTRACTOR SHOULD NOTE THAT ADDITIONAL WORK, NOT SHOWN OR NOTED ON THE PLANS, MAY BE REQUIRED AS THE CONTRACT PROGRESSES. THIS WORK SHALL BE PERFORMED BY THE CONTRACTOR AS ORDERED BY THE ENGINEER AND PAYMENT SHALL BE MADE AT THE UNIT PRICE BID FOR THE APPROPRIATE ITEM.
8. NO PAYMENT SHALL BE MADE FOR WORK CALLED FOR BY NOTES ON THE PLANS, IN THE SPECIFICATIONS, OR UNDER THE HEADING "GENERAL NOTES" UNLESS PAYMENT IS SPECIFICALLY INDICATED BY ITEM NUMBER. THE COST OF WORK FOR WHICH NO PAYMENT IS INDICATED SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE VARIOUS ITEMS IN THE CONTRACT.
9. THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN THE PROPERTY OF THE COUNTY WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN THE PROPERTY OF THE COUNTY, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED AT THE EXPENSE OF THE CONTRACTOR IN A MANNER SATISFACTORY TO THE ENGINEER.
10. THE CONTRACTOR IS ADVISED THAT ADDITIONAL NOTES WILL BE FOUND ON SUBSEQUENT SHEETS OF THE PLANS AND SUCH NOTES, WHILE PERTAINING TO THE SPECIFIC DRAWINGS THEY ARE PLACED ON, ALSO SUPPLEMENT THE GENERAL NOTES LISTED HEREIN.
11. WHENEVER ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE REMOVED AND DISPOSED OF, THE COST OF SUPPLYING A DISPOSAL AREA AND OF TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THOSE ITEMS.
12. THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY SUPPORTS, BRACING OR OTHER DEVICES THAT MAY BE REQUIRED, OR THAT MAY BE DIRECTED BY THE ENGINEER, TO PROTECT THE SAFETY OF ADJACENT STRUCTURES, ROADWAYS OR UTILITIES. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
13. ALL DISTURBED UNPAVED AREAS WITHIN THE DESIGNATED WORK LIMITS SHALL BE GRADED, FERTILIZED, SEEDED AND MULCHED AS SPECIFIED UNDER SECTION 610 (TURF ESTABLISHMENT), OF THE STANDARD SPECIFICATIONS.
14. AREAS DISTURBED BY THE CONTRACTOR THAT ARE OUTSIDE THE DESIGNATED WORK LIMITS SHALL BE GRADED IN A MANNER APPROVED BY THE ENGINEER AND SEEDED AS SPECIFIED FOR ITEM 610.0203. THE COST FOR THE RESTORATION OF AREA OUTSIDE THE WORK LIMITS SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE. THE CONTRACTOR'S ATTENTION IS DIRECTED TO SECTION 107-08 (PROTECTION OF RESTORATION OF PROPERTY AND LANDSCAPE).
15. THE METHOD OF REMOVAL OF EXISTING ROADWAY OR SHOULDER PAVEMENT IN THE IMMEDIATE VICINITY OF ANY UNDERGROUND UTILITIES, INCLUDING DRAINAGE STRUCTURES, SHALL BE REVIEWED WITH THE ENGINEER PRIOR TO COMMENCEMENT OF WORK.

16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR GUARDING AND PROTECTING ALL OPEN EXCAVATIONS IN ACCORDANCE WITH THE PROVISION OF SECTION 170-05 (SAFETY AND HEALTH REQUIREMENTS) OF THE NYS DOT STANDARD SPECIFICATIONS.
17. IF THE ENGINEER NOTIFIES THE CONTRACTOR OF ANY HAZARDOUS CONSTRUCTION PRACTICES, ALL OPERATIONS IN THE AFFECTED AREA SHALL BE DISCONTINUED AND IMMEDIATE ACTION SHALL BE TAKEN TO CORRECT THE SITUATION TO THE SATISFACTION OF THE ENGINEER BEFORE WORK IS RESUMED.
18. UPON NOTIFICATION BY THE ENGINEER, THE CONTRACTOR SHALL SUSPEND HIS OPERATIONS, AS NECESSARY, AND SHALL PROVIDE SAFE AND ADEQUATE ACCESS INTO OR THROUGH THE WORK SITE BY EMERGENCY VEHICLES.
19. THE RIGHT-OF-WAY SHOWN ON THE PLANS HAS BEEN TAKEN FROM INFORMATION CONTAINED IN CURRENT RECORDS OF THE ALBANY COUNTY DEPARTMENT OF PUBLIC WORKS. THE EXISTING RIGHT-OF-WAY HAS BEEN ASSUMED TO BE THREE RODS IN WIDTH, SYMMETRICAL ABOUT THE CENTERLINE OF THE EXISTING ROADWAY. WHEN CONSTRUCTION ACTIVITY IS EXPECTED TO BE AT OR NEAR THE RIGHT-OF-WAY LIMITS SHOWN, THE CONTRACTOR IS TO NOTIFY THE A.C.D.P.W. TO HAVE THEM DETERMINE THE STATED RIGHT-OF-WAY.
20. ALL EXISTING TRAFFIC SIGNS REMOVED BY THE CONTRACTOR SHALL BECOME THE PROPERTY OF THE ALBANY COUNTY DEPARTMENT OF PUBLIC WORKS. THE CONTRACTOR SHALL DELIVER REMOVED SIGNS TO THE SIGN SHOP AT THE ALBANY COUNTY DEPARTMENT OF PUBLIC WORKS, VOORHEESVILLE OFFICE.
21. PRIOR TO COMMENCEMENT OF ANY WORK, THE CONTRACTOR SHALL SUBMIT A PROPOSED CONSTRUCTION SEQUENCE TO THE ENGINEER FOR APPROVAL.
22. ALL PROPERTY CORNERS AND R.O.W. MONUMENTS DISTURBED BY CONTRACTOR SHALL BE RESET BY A N.Y.S. LICENSED SURVEYOR AT NO ADDITIONAL EXPENSE TO ALBANY COUNTY.

ASPHALT CONCRETE MINIMUM PRODUCTION

ALBANY COUNTY WILL REQUIRE THE SUCCESSFUL BIDDER TO PLACE A MINIMUM OF 1,000 TONS OF ASPHALT PER DAY. SUCCESSFUL BIDDER MUST RECEIVE PRIOR APPROVAL FROM ALBANY COUNTY TO DEVIATE FROM THIS MINIMUM REQUIREMENT. FAILURE TO MEET THESE STANDARDS WILL RESULT IN A PENALTY OF \$ 1,000 PER DAY.

UNDER CONTEMPLATION OF THIS CONTRACT, CONTRACTOR TO BE ADVISED THAT ALL MATERIALS GENERATED UNDER ITEM #490.10 ARE THE SOLE PROPERTY OF ALBANY COUNTY AND ALL RESPONSIBILITIES FOR REMOVAL AND STORAGE SHALL BE MET ACCORDINGLY.

CONTRACTOR SHALL BE AWARE THAT ALBANY COUNTY IS SPECIFYING 70 SERIES COMPACTION UNDER THIS CONTRACT AND THE CONTRACTOR IS RESPONSIBLE FOR ALL TESTING REQUIREMENTS IN THE APPLICABLE SPECIFICATION.

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COUNTY OF ALBANY
 DEPARTMENT OF PUBLIC WORKS
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REMARKS	GENERAL NOTES		DRAWING NUMBER: GN-1
BY	HAULING & PLACING ASPHALT CONCRETE ON VARIOUS COUNTY ROADS HIGHWAY IMPROVEMENT PROJECT		
DATE	DRAWN BY: ADF	SCALE: AS SHOWN	SHEET 2 OF 4
REV. #	CHECKED BY: BRM	DATE: 2025	
	APPROVED BY: BRM	PROJ. I.D. NO.: 25-C591	

FILE NAME: DATE: BY: PLOT SCALE:

STANDARDS

- PEDESTRIAN AND VEHICLE TRAFFIC SHALL BE MAINTAINED AND PROTECTED FOR THE LENGTH AND DURATION OF THIS CONTRACT, COMPLYING WITH THE REQUIREMENTS OF SECTION 619, WORK ZONE TRAFFIC CONTROL, OF THE NYS DOT STANDARD SPECIFICATIONS, THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), AND THESE PLANS. ALL SIGNS AND OTHER TRAFFIC CONTROL DEVICES USED ON THIS PROJECT SHALL CONFORM TO THE MUTCD.
- "NYS DOT STANDARD SPECIFICATIONS" REFERS TO THE VERSION IN EFFECT AS OF THE PROJECT LETTING DATE. "MUTCD" REFERS TO THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, AND THE NEW YORK STATE SUPPLEMENT TO THE NATIONAL MUTCD, IN EFFECT AS OF THE PROJECT LETTING DATE.

TRAFFIC CONTROL PLAN

- PRIOR TO THE START OF WORK, THE CONTRACTOR SHALL SUBMIT FOR APPROVAL AN OUTLINE OF HIS PROPOSED METHODS AND MANNER OF EXECUTING THE WORK, INCLUDING SEQUENCES OF OPERATION AND TIME SCHEDULE.

THE ENGINEER MAY REQUIRE THE CONTRACTOR TO SUBMIT A DETAILED WORK ZONE TRAFFIC CONTROL PLAN FOR REVIEW UNDER ANY OF THE FOLLOWING CONDITIONS:

- FOR ANY WORK NOT ADEQUATELY COVERED BY THE TYPICAL APPLICATIONS IN MUTCD SECTION 6H.
- IF THE CONTRACTOR PROPOSES TO MAINTAIN TRAFFIC ON AN UNPAVED SURFACE.
- WHEN THE CONTRACTOR PROPOSES A CHANGE THAT ALTERS THE BASIC CONCEPTS OF THE PLAN.
- AT THE DISCRETION OF THE ENGINEER.

THE PLAN SHALL DETAIL THE LOCATION OF ALL SIGNS, BARRICADES, LIGHTS, BARRELS, CONES, DELINEATORS, PAVEMENT MARKINGS, GUIDE RAIL AND OTHER TRAFFIC CONTROL DEVICES, AND SHALL COMPLY WITH THE STANDARD SPECIFICATIONS AND MUTCD. THE PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL TWO WEEKS IN ADVANCE OF THE BEGINNING OF WORK.

- AT ANY TIME DURING THE DURATION OF THE CONTRACT, THE CONTRACTOR MAY SUBMIT A REVISED WORK ZONE TRAFFIC CONTROL PLAN TO THE ENGINEER, FOR HIS APPROVAL.

- IF, IN THE OPINION OF THE ENGINEER, CONSTRUCTION WORKERS FACE UNDUPLICATE HAZARDS FROM WORK STOPS OR EQUIPMENT, THE CONTRACTOR SHALL SUBMIT AN INTERNAL TRAFFIC CONTROL PLAN, SHOWING PRACTICES AND PROCEDURES TO MINIMIZE RISK OF INJURY.

- UNLESS OTHERWISE SPECIFIED, THE CONTRACTOR SHALL MAINTAIN AND PROTECT TWO-WAY TRAFFIC ON ALL EXISTING ROADWAYS WITHIN THE PROJECT WORK LIMITS FOR THE DURATION OF THE CONTRACT. DURING NON-WORKING HOURS, TWO-WAY TRAFFIC SHALL BE MAINTAINED ON ALL TRAVELED WAYS. DURING WORK HOURS, ALTERNATING ONE-WAY TRAFFIC MAY BE MAINTAINED WITHIN THE CONTRACT LIMITS. ALTERNATING ONE-WAY TRAFFIC SHALL BE CONTROLLED BY FLAGGERS, MUTCD-COMPLIANT AUTOMATED FLAGGER ASSISTANCE DEVICES, OR TEMPORARY SIGNALS.

- THE CONTRACTOR SHALL MAINTAIN AND PROTECT TRAFFIC BY CONDUCTING HIS WORK OPERATIONS SO THAT THE PUBLIC IS SUBJECTED TO A MINIMUM OF DELAY AND HAZARD.

- SIMULTANEOUS WORK ON BOTH SIDES OF THE TRAVELED WAY AT THE SAME LOCATION IS NOT PERMITTED UNLESS ALLOWED BY THE ENGINEER.

- THE CONTRACTOR SHALL PROVIDE PERSONNEL TO PATROL THE CONTRACT AREA AS NECESSARY TO ENSURE THAT CONDITIONS ON THE SITE ARE ADEQUATE FOR PUBLIC SAFETY AND CONVENIENCE AT ALL TIMES. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

- THE CONTRACTOR SHALL PROTECT FROM DAMAGE ANY PAVEMENTS, CURBS, SIDEWALKS, ETC. THAT ARE TO REMAIN AFTER THE PROJECT. HE SHALL OBSERVE ALL RULES, REGULATIONS AND DIRECTIONS OF THE MAINTAINING AGENCY WHEN MOVING MATERIAL OR EQUIPMENT OUTSIDE OF PROJECT LIMITS AND TAKE SUCH PROTECTIVE MEASURES AS MAY BE ORDERED BY THE ENGINEER. PAVEMENTS, CURBS AND SIDEWALKS, ETC., INSIDE OR OUTSIDE THE PROJECT LIMITS THAT ARE NOT SCHEDULED TO BE REPLACED AND ARE DAMAGED BY THE CONTRACTOR THROUGH DISREGARD OF THESE PROVISIONS, SHALL BE RESTORED AT HIS SOLE EXPENSE.

- RESPONSIBILITY FOR EMERGENCY REPAIRS: THE CONTRACTOR SHALL IN WRITING, SUBMIT TO THE POLICE DEPARTMENT HAVING JURISDICTION AND THE NEW YORK STATE POLICE THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE PERSON OR PERSONS AUTHORIZED TO SECURE LABOR, MATERIALS AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE OF NORMAL WORKING HOURS. DUPLICATE COPIES OF THE ABOVE SHALL BE FILED WITH THE ENGINEER.

TRAFFIC CONTROL DEVICES

- ALL SIGNS, BARRICADES OR LIGHTED BARRICADES AND OTHER DEVICES REQUIRED FOR THIS PROJECT SHALL BE INCLUDED IN PAYMENT FOR ITEM 619.01, UNLESS OTHERWISE SPECIFIED. ALL SIGNS SHALL BE MADE WITH ASTM TYPE III SHEETING (HIGH INTENSITY) OR BETTER.

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL TEMPORARY TRAFFIC CONTROL DEVICES, MAKING SURE ALL SIGNS, CONES, BARRICADES, ETC., ARE IN PLACE AND IN GOOD CONDITION. THE SOLE JUDGE OF THE EFFECTIVENESS OF THE CONTRACTOR'S EFFORTS TOWARD PROTECTION OF TRAFFIC AND PERSONNEL SHALL BE THE ENGINEER.

- DEVICES SHALL BE KEPT IN PROPER POSITION, CLEAN, AND LEGIBLE, REGARDLESS OF VARYING CONDITIONS. DAMAGED, DEFACED, OR DIRTY DEVICES SHOULD BE CLEANED, REPAIRED OR REPLACED. WHERE WORK IS ACCOMPLISHED IN STAGES, ONLY THOSE DEVICES APPLICABLE TO THE STAGE IN PROGRESS SHOULD BE DISPLAYED. DEVICES NO LONGER APPLICABLE SHOULD BE PROMPTLY REMOVED OR COVERED. DEVICES SHOULD BE KEPT CURRENT (FOR EXAMPLE, WORKER SIGNS SHOULD BE REPLACED BY ROAD WORK SIGNS WHEN WORKERS ARE OFF DUTY, AND FLAGGER SIGNS SHALL BE COVERED OR TURNED FROM VIEW WHEN THE FLAGGER IS NOT ON DUTY.)

- VEGETATION, CONSTRUCTION MATERIALS, EQUIPMENT, VEHICLES OR WORKERS SHOULD NOT OBSCURE TRAFFIC CONTROL DEVICES, OR OBSTRUCT TRAFFIC.

- ALL DEVICES PLACED WITHIN THE CLEAR ZONE SHALL COMPLY WITH NCHRP 350 OR M.A.S.H. CRASH TESTING STANDARDS.

- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE PAVEMENT MARKINGS AT ALL TIMES ON THE PROJECT. THESE MARKINGS ARE FOR TRAFFIC CONTROL ONLY AND SHALL CONFORM TO THE REQUIREMENTS OF ITEM 619.09(1), TEMPORARY PAVEMENT MARKINGS - TRAFFIC PAINT.

- IT MAY BE NECESSARY TO MARK, REMARK, ALTER AND OBLITERATE PAVEMENT MARKINGS ON TEMPORARY AND PERMANENT PAVEMENT WHERE ORDERED BY THE ENGINEER. MARKINGS SHALL BE REMOVED BY MEANS WHICH COMPLETELY REMOVE ALL TRACES OF PAINT OR PLASTIC STRIPS. ON FINAL TOP COURSE PAVEMENT, A METHOD THAT WILL MINIMIZE SCARING OF THE PAVEMENT SHALL BE USED. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

- DELINEATION SHALL BE USED ALONG EMBANKMENTS AND OTHER LOCATIONS WHERE THE EXISTING GUIDE RAIL HAS BEEN REMOVED AND SHALL REMAIN UNTIL SATISFACTORY PROTECTION IS PROVIDED. SPACING OF THE DELINEATION DEVICES SHALL BE CONSISTENT WITH THE CONTRACT DOCUMENTS AND AS ORDERED BY THE ENGINEER. BARRELS OR VERTICAL PANELS MAY BE USED, EXCEPT AT INTERSECTIONS OR OTHER LOCATIONS WHERE VERTICAL PANELS MAY BE VIEWED EDGE ON, IN WHICH CASE BARRELS SHALL BE USED.

INCLEMENT WEATHER AND NIGHT WORK

- IF WORK AFTER DARK IS ALLOWED, THE REQUIREMENTS OF MUTCD SECTION 6F.7B, WARNING LIGHTS, AND NYS DOT STANDARD SPECIFICATIONS SECTION 619-3.19 SHALL APPLY. ANY MUNICIPAL NOISE ORDINANCES SHALL BE OBSERVED.

- WHEN THE VISIBILITY OF THE TRAVELING PUBLIC IS RESTRICTED DUE TO WEATHER CONDITIONS AND/OR THE WORK OPERATION COMMENCES PRIOR TO DAWN OR CONTINUES BEYOND DUSK, FLASHING BEACONS AND STEADY BURNING LIGHTS WILL BE PROVIDED BY THE CONTRACTOR AND PLACED AS DIRECTED BY THE ENGINEER.

- THE CONTRACTOR MUST SCHEDULE HIS OPERATIONS IN SUCH A MANNER THAT A HARD-SURFACED PAVEMENT WILL BE PROVIDED FOR TWO-WAY TRAFFIC DURING THE WINTER MONTHS OR AT ANY TIME THE CONTRACT IS SUSPENDED IN EXCESS OF TWO WEEKS. HARD-SURFACED PAVEMENT SHALL BE CONSIDERED THE EXISTING PAVEMENT OR THE NEW PAVEMENT COMPLETED TO AT LEAST THE BINDER COURSE.

SAFETY

- IF THE ENGINEER NOTIFIES THE CONTRACTOR OF ANY HAZARDOUS CONSTRUCTION PRACTICES, ALL OPERATIONS IN THAT AREA SHALL BE DISCONTINUED AND IMMEDIATE REMEDIAL ACTION SHALL BE TAKEN TO CORRECT THE SITUATION TO THE SATISFACTION OF THE ENGINEER BEFORE WORK IS RESUMED.

- THE CONTRACTOR SHALL NEITHER PARK EQUIPMENT NOR STORE MATERIAL OVERNIGHT WHERE IT IS DEEMED BY THE ENGINEER TO BE A SAFETY HAZARD TO TRAFFIC. THIS INCLUDES AREAS WITHIN THE DEFLECTION ZONE OF GUIDE RAIL.

- VEHICLES BELONGING TO THE CONTRACTOR OR SUBCONTRACTORS, OR EMPLOYEES OF THE CONTRACTOR OR SUBCONTRACTORS SHALL NOT BE PARKED ON THE PAVEMENT OR SHOULDER ALONG OR ADJACENT TO TRAVEL LANES OPEN TO TRAVEL WITHIN THE PROJECT LIMITS.

FLAGGING

- IF IN THE OPINION OF THE ENGINEER, TRAFFIC EXCEEDS THE VOLUME THAT FLAGGERS CAN SAFELY HANDLE, PEAK HOUR LANE CLOSURE RESTRICTIONS MAY BE IMPOSED.

- FLAGGERS SHALL BE TRAINED IN PROPER FLAGGING TECHNIQUES. IF IN THE OPINION OF THE ENGINEER, THE COUNTY TRAFFIC ENGINEER, OR THE COUNTY SAFETY OFFICER, A FLAGGER IS NOT USING PROPER TECHNIQUES, THE CONTRACTOR SHALL RELIEVE THE FLAGGER IMMEDIATELY UNTIL HE OR SHE IS RETRAINED.

- THE CONTRACTOR MAY SUBSTITUTE PORTABLE TRAFFIC SIGNALS OR AUTOMATED FLAGGER ASSISTANCE DEVICES FOR FLAGGERS. SIGNALS SHALL MEET THE STANDARDS OF THE MUTCD AND STANDARD SPECIFICATIONS SECTION 619-3.02. AUTOMATED FLAGGER ASSISTANCE DEVICES SHALL COMPLY WITH MUTCD SECTION 63.04. AN INSTALLATION PLAN AND TIMING PLAN MUST BE SUBMITTED FOR REVIEW TO THE COUNTY TRAFFIC ENGINEER AT LEAST 10 WEEKDAYS BEFOREHAND.

FLAGGING STATIONS

- FLAGGING STATIONS MAY BE ESTABLISHED, AND FLAGGERS ASSIGNED, TO POSITIVELY ASSIGN RIGHT OF WAY BY SIGNALING DRIVERS TO STOP OR PROCEED, OR TO MAINTAIN PROPER TRAFFIC FLOW PAST A WORK AREA BY SIGNALING DRIVERS TO REDUCE SPEED.

- FLAGGING STATIONS SHOULD BE POSITIONED ADJACENT TO THE TRAFFIC LANES BEING CONTROLLED, AT LOCATIONS THAT PROVIDE DRIVERS WITH SUFFICIENT STOPPING SIGHT DISTANCE TO PERMIT PROPER RESPONSE TO THE FLAGGER'S SIGNALS, SEE MUTCD TABLE 6E-1: STOPPING SIGHT DISTANCE AS A FUNCTION OF SPEED.

ONE-PERSON FLAGGING. FOR A SHORT ONE-LANE TWO-WAY SECTION PAST A SMALL WORK AREA, ONE FLAGGING STATION MAY BE ADEQUATE. THE FLAGGER SHOULD BE LOCATED ON THE SHOULDER OPPOSITE THE WORK AREA. IF TRAFFIC AVERAGES MORE THAN ONE VEHICLE PER MINUTE, ONE-PERSON FLAGGING SHOULD NOT BE USED.

TWO-PERSON FLAGGING. FOR LONGER WORK AREAS OR HIGHER VOLUMES, FLAGGING STATIONS MAY BE NECESSARY IN ADVANCE OF EACH END OF THE WORK AREA. ADDITIONAL FLAGGERS SHALL BE USED AS NEEDED AT INTERSECTIONS OR DRIVEWAYS IN THE ONE-LANE SECTION.

LANE REDUCTION. ON MULTILANE ROADS, A FLAGGER MAY BE STATIONED NEAR THE BEGINNING OF THE LANE CLOSURE TAPER, OR AT AN APPROPRIATE DISTANCE IN ADVANCE OF THE WORK AREA, TO SIGNAL TRAFFIC TO SLOW DOWN.

OTHER FACTORS. FLAGGING STATIONS SHOULD NOT BE LOCATED IN THE TRAFFIC LANES BEING USED BY TRAFFIC, WHERE WORKERS GATHER, OR WHERE THE HIGH VISIBILITY CLOTHING WORN BY THE FLAGGER WOULD NOT CONTRAST WITH THE BACKGROUND. FLAGGING STATIONS SHOULD ALSO HAVE ESCAPE ROUTES IN CASE OF ERRANT VEHICLES.

- THE FLAGGER SHALL USE PROPER EQUIPMENT, AS DESCRIBED IN MUTCD SECTION 6E. IN PARTICULAR:

EACH FLAGGER SHALL WEAR HIGH VISIBILITY CLOTHING COMPLYING WITH MUTCD SECTION 6E.02. THE CLOTHING SHALL BE WORN OUTSIDE ALL OTHER CLOTHING. CLASS 2 OR CLASS 3 RETROREFLECTIVE CLOTHING IS REQUIRED DAY AND NIGHT.

APPROPRIATE HAND SIGNALING DEVICES SHALL BE USED (SEE MUTCD SECTION 6E.03). STOP/SLOW PADDLES ARE THE PREFERRED DEVICE. RED FLAGS SHALL BE AT LEAST 24 INCHES SQUARE. ORANGE FLAGS ARE PROHIBITED.

SIGNS AS SHOWN IN FIGURE 1 SHALL BE PLACED ON ALL APPROACHES TO ANY FLAGGING STATION IN USE MORE THAN BRIEFLY. A FLAGGER MAY BRIEFLY CONTROL TRAFFIC WITHOUT FLAGGER ADVANCE WARNING SIGNS.

WHEN MAINTAINING ALTERNATING ONE-WAY TRAFFIC, FLAGGERS SHALL USE RADIOS OR FIELD TELEPHONES IF NECESSARY IN THE OPINION OF THE ENGINEER, OR IF VOICE OR VISUAL COMMUNICATION BETWEEN FLAGGING STATIONS IS NOT PRACTICAL. THE COST OF ANY RADIO OR FIELD TELEPHONES USED SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

FLAGGERS USED DURING NON-DAYLIGHT HOURS SHALL BE ILLUMINATED BY OVERHEAD FLOODLIGHTS. SIGNALING DEVICES SHALL BE RETROREFLECTIVE IF USED AT NIGHT.

FLAGGER'S DUTIES

THE FLAGGER'S PRIORITIES, IN ORDER OF IMPORTANCE, ARE THE SAFETY OF THE TRAVELING PUBLIC AND CONSTRUCTION WORKERS, EFFICIENT TRAFFIC FLOW, AND CONSTRUCTION EFFICIENCY.

RELIABILITY. THE FLAGGER SHOULD REMAIN ON DUTY AT A FLAGGING STATION UNTIL RELIEVED BY THE WORK ZONE TRAFFIC CONTROL SUPERVISOR, OR A REPLACEMENT FLAGGER. FLAGGERS SHOULD BE PROVIDED WITH SUFFICIENT REST AND MEAL BREAKS SO THE FLAGGER CAN REMAIN ALERT.

ALERTNESS. THE FLAGGER SHOULD BE ALERT, RESPONSIVE TO SHIFTING WORK AREA AND WORKER LOCATIONS, AND REMAIN IN A POSITION TO EFFECTIVELY CONTROL TRAFFIC APPROACHING AND PASSING THE WORK AREA. WHEN STATIONED TO SIGNAL FOR REDUCED SPEED, THE FLAGGER SHOULD CONCENTRATE ON THE NEED FOR SPEED REDUCTION BY APPROACHING TRAFFIC, AND SHOULD CONTINUE SIGNALING AS NECESSARY TO ACHIEVE MOTORIST COMPLIANCE.

CONTROL. FLAGGERS CONTROLLING ONE DIRECTION OF TRAFFIC SHOULD FACE APPROACHING TRAFFIC. FLAGGERS CONTROLLING TWO DIRECTIONS OF TRAFFIC SHOULD BE ALERT TO TRAFFIC ON EACH APPROACH. FLAGGERS CONTROLLING TRAFFIC AT INTERSECTIONS SHOULD BE SURE THAT TRAFFIC ON ALL CONFLICTING APPROACHES IS HALTED BEFORE ASSIGNING RIGHT-OF-WAY TO A PARTICULAR APPROACH.

SIGNALS. THE FLAGGER SHOULD CONTROL TRAFFIC CLEARLY AND AUTHORITATIVELY BY SIGNALING STOP, PROCEED OR REDUCE SPEED. THE FLAGGER SHOULD KNOW AND USE STANDARD SIGNALING PROCEDURES (MUTCD SECTION 6E.04). TRAFFIC SHOULD BE GRANTED THE RIGHT-OF-WAY EXPEDITIOUSLY, TO AVOID UNREASONABLE DELAYS.

COORDINATION. THE FLAGGER SHALL COORDINATE RIGHT-OF-WAY ASSIGNMENT WITH OTHER FLAGGERS, TRAFFIC SIGNALS, AND HIGHWAY CONDITIONS, AS NECESSARY. IF POSSIBLE, TRAFFIC SIGNALS SHOULD BE TURNED OFF WHILE FLAGGERS ARE CONTROLLING THE INTERSECTION.

WARNING. THE FLAGGER SHOULD, WHEN NECESSARY, WARN OTHER WORKERS TO KEEP AWAY FROM THE FLAGGING STATION, OR TO MOVE AWAY FROM THE TRAFFIC PATH. THE FLAGGER SHOULD WARN CO-WORKERS OF IMMINENT DANGER, SUCH AS AN ERRANT VEHICLE.

CONCENTRATION. THE FLAGGER SHOULD REFRAIN FROM OTHER WORK INVOLVEMENT, OR DISTRACTING CONVERSATIONS WITH PASSING MOTORISTS OR OTHER WORKERS.

COURTESY. THE FLAGGER SHOULD TREAT THE PUBLIC WITH COURTESY AND RESPECT.

DROP-OFFS AND EXCAVATIONS NEAR TRAVELED WAYS

- AT LOCATIONS WHERE THE PAVEMENT MUST BE REMOVED AND TRAFFIC CANNOT BE DIVERTED AROUND THE LOCATION, THE OPENING SHALL BE BACKFILLED AND BROUGHT TO GRADE WITH 2 1/2" OF ASPHALT CONCRETE PRIOR TO THE RESUMPTION OF THE DAY'S OPERATIONS. THE COST OF BACKFILL ABOVE THEORETICAL SUBGRADE AND THE COST OF THE ASPHALT CONCRETE SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

- ALL WORK SHALL BE SCHEDULED TO MINIMIZE THE DEPTH AND DURATION OF DROP-OFFS ALONG THE PAVEMENT EDGE BY PLACING SUCCESSIVE PAVEMENT COURSES AND SHOULDER MATERIAL TO FILL ALONG THE PAVEMENT EDGE.

- WHERE EXCAVATION PRODUCES DROP-OFFS NEAR THE TRAVELED WAY, CHANNELIZATION SHALL BE PROVIDED IN ACCORDANCE WITH TABLE 3, BELOW. NO DROP-OFF GREATER THAN FOUR INCHES SHALL REMAIN OVER A WEEKEND OR HOLIDAY. A DROP-OFF IS CONSIDERED ELIMINATED IF TAPERED AWAY BY A 1 ON 4 SLOPE OR FLATTER. A MINIMUM OF TWO CHANNELIZING DEVICES SHALL BE USED FOR EACH DROP-OFF THAT IS NOT ELIMINATED BY A 1 ON 4 OR FLATTER SLOPE.

- DROP-OFFS NEAR SIDEWALKS SHALL BE PROTECTED WITH A CONTINUOUS BARRIER IN ACCORDANCE WITH SECTION 619-3.02 C.

- DROP-OFFS ON BOTH SIDES OF THE TRAVELED WAY AT THE SAME TIME SHALL NOT BE PERMITTED. SHOULDER AREAS SHOULD BE PREPARED TO RECEIVE THE SHOULDER PAVEMENT MATERIAL IMMEDIATELY AHEAD OF THE SHOULDER PAVING TO MINIMIZE THE TIME A DROP-OFF EXISTS. NO SHOULDER (NYW4-13) SIGNS SHALL BE ERRECTED AT NO MORE THAN 1/4 MILE APART THROUGHOUT THE PROJECT WHERE A DROP-OFF EXISTS.

- AT LOCATIONS WHERE THE CONTRACTOR REMOVES THE EXISTING PAVEMENT AND THE NEW PAVEMENT COURSES ARE NOT YET PLACED, THE EXCAVATION AREA ABOVE SUBGRADE SHALL BE BROUGHT TO THE GRADE OF THE EXISTING PAVEMENT BY THE END OF THE WORK DAY WITH MATERIAL MEETING THE REQUIREMENTS OF ITEM 304.12, SUBBASE COURSE TYPE 2. THE COST OF THIS WORK AND THE EVENTUAL EXCAVATION OF THIS MATERIAL TO THE BOTTOM OF THE ASPHALT PAVEMENT COURSE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL. ANY REPAIRS NECESSARY TO THESE AREAS SHALL BE MADE AS NEEDED AND DIRECTED BY THE ENGINEER. IF THE GRANULAR SURFACE SHOULD BE CONSIDERED INAPPROPRIATE FOR EXISTING SPEED LIMIT, ROUGH ROAD (WB-8) SIGNS SUPPLEMENTED BY ADVISORY SPEED (W13-1) SIGNS SHALL BE INSTALLED AT THESE LOCATIONS. IN ADDITION, PAVEMENT ENDS (WB-3) SIGNS AND FLASHING BEACONS SHALL BE PROVIDED. THE ENTIRE LENGTH SHALL BE DELINEATED ALONG THE EDGE OF THE TRAVEL LANES AS DIRECTED BY THE ENGINEER.

ACCESS TO ADJACENT LANDS AND ROADWAYS

- THE COST OF PROVIDING AND MAINTAINING SAFE AND ADEQUATE ACCESS TO AND FROM INTERSECTING HIGHWAYS, HOMES AND BUSINESSES AT ALL TIMES TO THE SATISFACTION OF THE ENGINEER, SHALL BE BORNE BY THE CONTRACTOR. ANY TEMPORARY ASPHALT CONCRETE PLACED TO MAINTAIN THIS ACCESS AS DIRECTED BY THE ENGINEER SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 619.01.

- THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR PROVIDING SAFE AND ACCESSIBLE PEDESTRIAN ACCESS AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE AN ACCESSIBLE WALKWAY FOR PEDESTRIAN TRAFFIC ANY TIME AN A.D.A. ACCESSIBLE ROUTE IS DISRUPTED BY CONSTRUCTION ACTIVITIES.

- THE CONTRACTOR SHALL SCHEDULE HIS OPERATION SO TO LEAVE ONE DRIVEWAY OPEN FOR EACH BUSINESS IN THE PROJECT AREA AT ALL TIMES. AT ANY TIME THE CONTRACTOR IS NOT WORKING, ALL DRIVEWAYS ON THE PROJECT SHALL BE OPEN TO TRAFFIC.

INSTALLATION AND REMOVAL OF TRAFFIC CONTROL DEVICES

- NECESSARY DEVICES SHOULD BE INSTALLED AT THE TIME THE WORK AREA IS ESTABLISHED.

INSTALLATION AND REMOVAL OF DEVICES, IN ITSELF, CONSTITUTES HIGHWAY WORK THAT SHOULD BE PROTECTED BY MEANS APPROPRIATE TO THE SITUATION.

- ORDER OF WORK
A. DEVICE INSTALLATION NORMALLY PROCEEDS FROM THE BEGINNING OF THE WORK ZONE TO THE FAR END. ON A TWO-WAY ROAD, THE LESS AFFECTED DIRECTION SHOULD BE SET UP BEFORE THE MORE AFFECTED ONE. THE WORK VEHICLE USED SHOULD MOVE IN THE SAME DIRECTION AS TRAFFIC IN THE ROADWAY HALF BEING SET UP.

- ON A ONE-WAY ROADWAY, DEVICE REMOVAL IS NORMALLY STARTED AT THE DOWNSTREAM END, WITH REMOVAL OF THE END ROAD WORK SIGN. CHANNELIZING DEVICES ARE THEN REMOVED, FROM DOWNSTREAM TO UPSTREAM, WITH THOSE IN ADVANCE OF THE WORKERS CONTINUING TO PROVIDE PROTECTION, UNTIL THE ROADWAY IS CLEARED. REMOVAL OF ADVANCE WARNING SIGNS ON THE SHOULDER IS NORMALLY ACCOMPLISHED WITH A WORK VEHICLE TRAVELING IN THE DIRECTION OF TRAFFIC.

- ON A TWO-WAY ROADWAY, DEVICE REMOVAL NORMALLY BEGINS WITH REMOVAL OF CHANNELIZING DEVICES THAT SEPARATE TRAFFIC FROM THE WORK AREA, FOLLOWED BY THOSE THAT SEPARATE OPPOSITE TRAFFIC. WARNING SIGNS ARE THEN REMOVED, STARTING WITH THE END ROAD WORK SIGN AND WORKING TOWARDS THE ROAD WORK AHEAD SIGN. THE WORK VEHICLE SHALL MOVE WITH THE FLOW OF TRAFFIC IN ITS HALF OF THE ROADWAY, WHERE TRAFFIC VOLUME IS LIGHT, WORKERS MAY CARRY SIGNS ACROSS THE ROAD TO A WORK VEHICLE ON THE OPPOSITE SIDE.

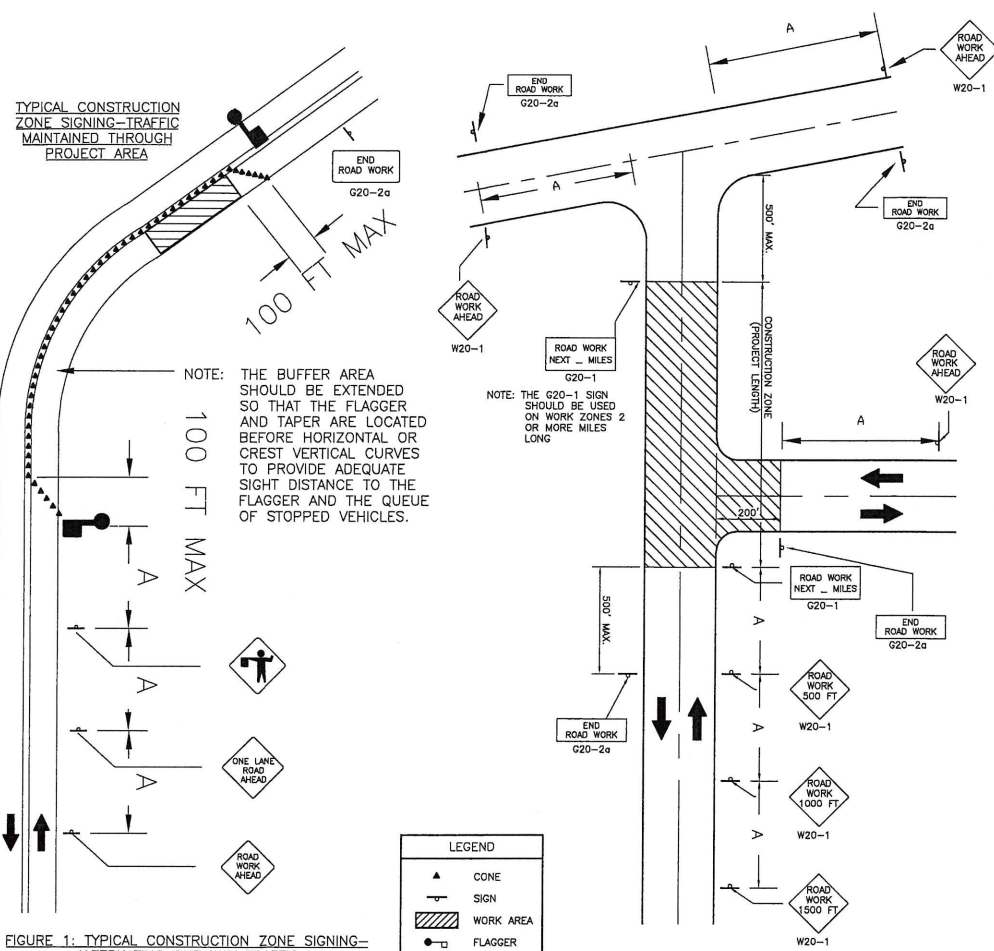


FIGURE 1: TYPICAL CONSTRUCTION ZONE SIGNING - ALTERNATING ONE WAY TRAFFIC. N.T.S. ONLY SIGNING IN ONE DIRECTION IS SHOWN. SIMILAR SIGNING SHALL BE USED IN THE OTHER DIRECTION.

TABLE 1: SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS 'A'
URBAN (≤30 MPH)	100 FT
URBAN (>30 MPH)	350 FT
SUBURBAN	500 FT
RURAL	500 FT


TABLE 2: CHANNELIZING DEVICE SPACING

POSTED SPEED (MPH)	MAXIMUM LONGITUDINAL SPACE (FT.) BETWEEN CHANNELIZING DEVICES	
	TAPER	TANGENT
30	30	60
35	35	70
40	40	80
45	45	90
50	50	100
55	55	110

TABLE 3: PAVEMENT EDGE DROP-OFF PROTECTION

DROP-OFF AT OR WITHIN SHOULDER				DROP-OFF OUTSIDE OF SHOULDER EDGE						
DROP-OFF HEIGHT	EDGE LINE PAVEMENT MARKINGS	DRUMS OR VERTICAL PANELS	TUBULAR MARKERS OR CONES	SIGNS	DROP-OFF HEIGHT	EDGE LINE PAVEMENT MARKINGS	SHOULDER CLOSURE	DRUMS OR VERTICAL PANELS	TUBULAR MARKERS OR CONES	SIGNS
WITHIN 4 FT FROM TRAVEL LANE				WITHIN 4 FT FROM TRAVEL LANE						
2 - 6 IN	YES	100 FT	NO	W21-5a SHLD CLSD	2 - 6 IN	YES	NO	100 FT	NO	W8-9 LOW SHLD
6 IN - 2 FT	YES	40 FT	NO	W21-5a SHLD CLSD	6 IN - 2 FT	YES	NO	40 FT	NO	W8-9 LOW SHLD
	NO	20 FT	NO	W21-5a SHLD CLSD		NO	YES	40 FT	NO	W21-5a SHLD CLSD
MORE THAN 4 FT FROM TRAVEL LANE				MORE THAN 4 FT FROM TRAVEL LANE						
2 - 6 IN	YES	200 FT	100 FT	W21-5a SHLD CLSD	2 - 6 IN	YES	NO	200 FT	100 FT	W8-9 LOW SHLD
6 IN - 2 FT	YES	40 FT	40 FT	W21-5a SHLD CLSD	6 IN - 2 FT	YES	NO	100 FT	40 FT	W21-5a SHLD CLSD
	NO	100 FT	40 FT	W21-5a SHLD CLSD		NO	YES	40 FT	NO	W21-5a SHLD CLSD

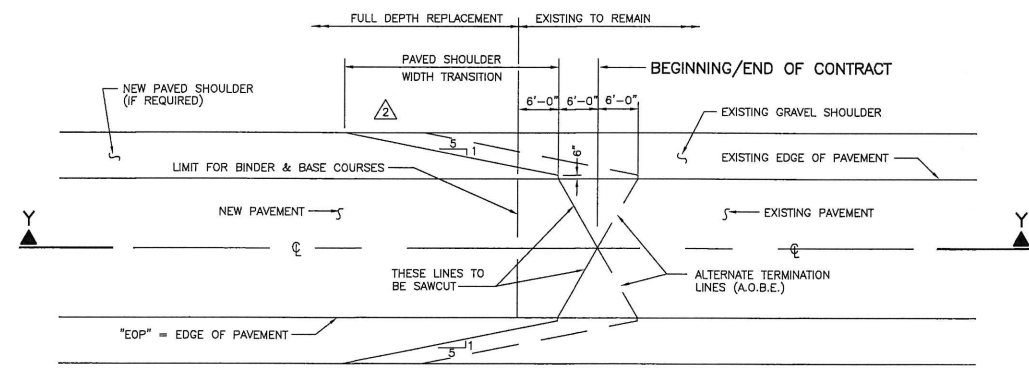
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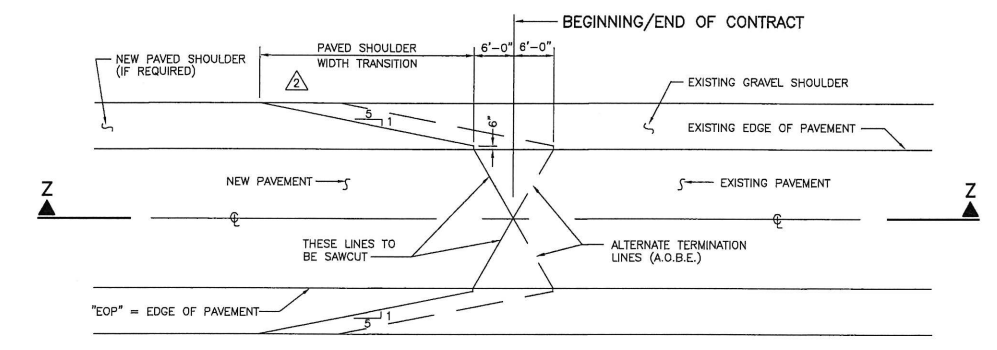
COUNTY OF ALBANY
DEPARTMENT OF PUBLIC WORKS
449 NEW SALEM ROAD
VOORHEESVILLE, NEW YORK 12186
518-765-2786

STANDARD DETAILS		FINAL PLANS COVER FOR STAMP
MAINTENANCE AND PROTECTION OF TRAFFIC FOR CONSTRUCTION PROJECTS		DRAWING NUMBER: ACS-11
DRAWN BY: JJM	SCALE: AS SHOWN	DRAWING NUMBER: ACS-11
CHECKED BY: BC	DATE: 2019	
APPROVED BY: WMA	PROJ. I.D. NO.: 2019-0342	

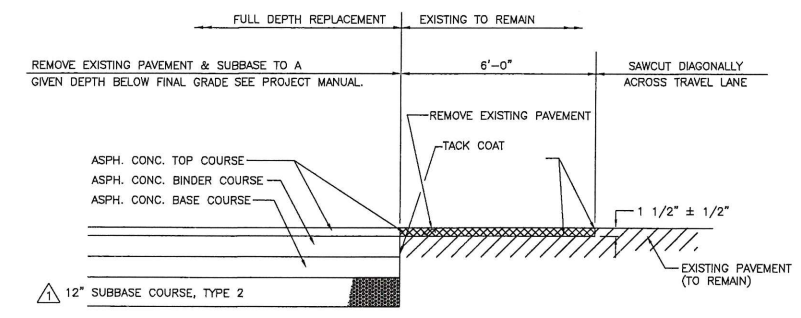
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PLAN



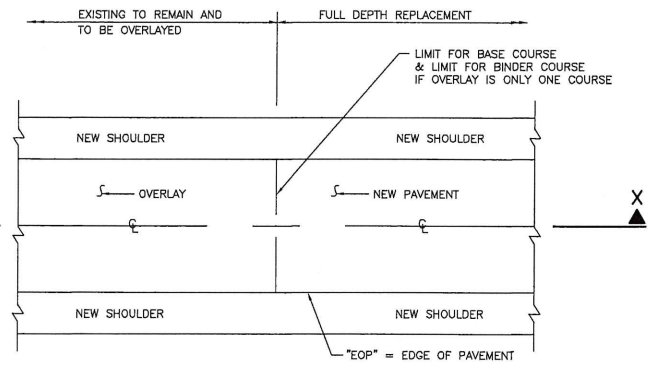
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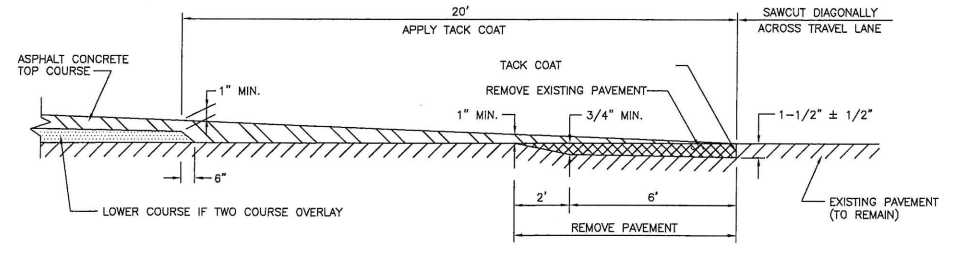
SECTION Y-Y

TERMINATION DETAIL FOR FULL DEPTH REPLACEMENTS

N.T.S.
PAYMENT FOR PAVEMENT TERMINATION WORK TO BE INCLUDED IN PRICE BID FOR ASPHALT CONCRETE TOP COURSE. NO DIRECT PAYMENT WILL BE MADE FOR SAWCUT OR REMOVAL OF MATERIAL.



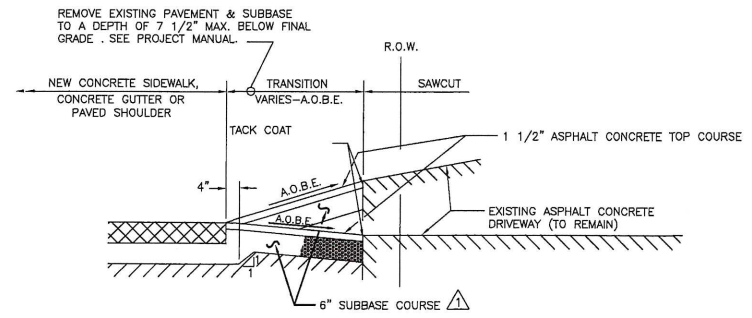
PLAN



SECTION Z-Z

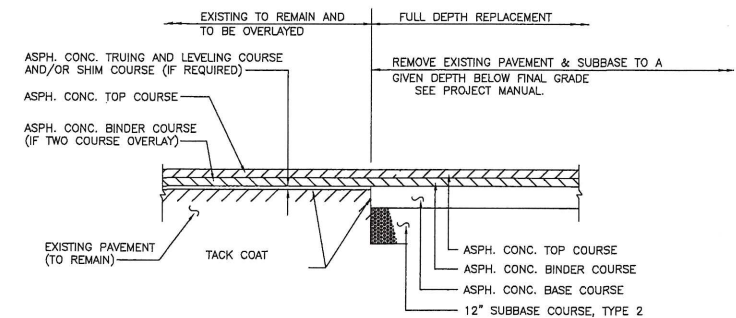
TERMINATION DETAIL FOR PAVEMENT OVERLAYS

N.T.S.
PAYMENT FOR PAVEMENT TERMINATION WORK TO BE INCLUDED IN PRICE BID FOR ASPHALT CONCRETE TOP COURSE. NO DIRECT PAYMENT WILL BE MADE FOR SAWCUT OR REMOVAL OF MATERIAL.



TERMINATION DETAIL AT EXISTING ASPHALT CONCRETE DRIVEWAYS

N.T.S.



SECTION X-X

PAVEMENT TRANSITION DETAIL PAVEMENT OVERLAYS TO FULL DEPTH RECONSTRUCTION

N.T.S.

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ADDED PAVED SHOULDER WIDTH TRANSITIONS		REMARKS	
FD	BY	ADDED PAVEMENT TRANSITIONS TO FULL DEPTH RECONSTRUCTION	
8/2/95		DATE	
3/14/91		DATE	
DRAWN BY: RFC		SCALE: AS SHOWN	
CHECKED BY: WAA		DATE:	
APPROVED BY: WAA		PROJ. I.D. NO.: 12-102	
STANDARD DETAILS			FINAL PLANS COVER FOR STAMP
PAVEMENT TERMINATION DETAILS			
DRAWING NUMBER: ACS-42			

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FILE NAME: D:\CIVIL\BAY\PLT\SCALE
BY: X/X/X